

SCRUTINY REQUIREMENTS

INTRODUCTION :

An entrant shall be deemed to have full knowledge of their car and to vouch for its eligibility by the act of their car being presented for scrutiny at the start.

The regulations should be interpreted on the basis that if it does not say you can do it then you cannot.

The details on the top of the MOTORSPORT IRELAND Scrutiny Sheet must be filled in and signed by the Entrant/Driver and all equipment including competition numbers and all advertising must be affixed before the car is presented to the scrutineers. If components or equipment is added or modifications are made after mechanical Scrutiny the vehicle must be represented for re-scrutiny. Cars must not be presented to scrutineers with warm engines and in addition must be presented at scrutiny in a clean and tidy condition, with presentable body and paintwork, ready to compete.

Scrutiny is passed only when a scrutiny sheet is signed by a Race scrutineer (or deputy) nominated for that class. All other events a scrutineer nominated for that event.

The onus rests with the competitor to enter a class for which their car is eligible. Classes are final at scrutiny.

Rallies and Navigation Trials: Competitors are reminded that their cars must comply with the Road Traffic Act at all times and that they must make all reasonable efforts to ensure this.

Race: Transponders to be fitted as low as possible maintaining line of site with the track this position being at the discretion of the class scrutineer. Only the current pass scrutiny sticker may be displayed.

1. HEADLAMPS, DRIVING LAMPS

1.1. Navigation Trials.

Standard headlights may only be supplemented by two additional lights. All such lights must extinguish on dipped beam. If additional lights are fitted, any supplementary manufacturer's lights must be removed. The wattage of all bulbs in forward facing lamps must comply with the RTA.

1.2. Stage Rally and Rally sprints.

The number of auxiliary lamps must comply with F.I.A. Year Book - (Groups A, N and B - Maximum 8). All auxiliary lamps must extinguish on the dipping system.

1.2.1. The use of Gas discharge lights on open road section of Stage Rallies is prohibited,

either as additional lights or as standard headlights, unless they are part of the original vehicle manufacturers specification.

1.3. Racing.

All lights to be taped.

2. BRAKE LIGHTS, INDICATORS, REVERSE LIGHTS.

2.1. Reverse Lights must only work when the gear lever is in the reverse position. (except Retro's where no provision exists for the switch in the Gearbox) Scrutineers must insist on any incorrect lamp being removed; this includes the rear lamp.

2.2. Racing & Rallycross - Brake & Rain Lights.

Saloons and Historic or Classic Cars must have 2 rearward facing brake lights with bulbs of 21 watts minimum. Brake lights clearly visible. (Rallycross, at high level in rear window) All cars must have a rearward facing red rain light of 21 watts minimum. All lights must be in working order.

3. STEERING, BRAKES, SUSPENSION.

3.1. Steering.

Must all be in very good condition. Steering freeplay will not be allowed. Scrutineers will particularly check universals, rubber couplings, the column shaft for any twisted splines and the condition of the steering wheel. Steering anti-theft device (steering lock) must be rendered inoperative by removal.

3.2. Brakes.

A good firm pedal is required, with no fluid leaks, no perished hoses, no pure copper or corroded pipes. Brake and clutch fluid reservoirs, if in the cockpit must be in a leakproof container **or of a type homologated by the FIA.**

3.2.1. Race / Hillclimb.

A Dual Circuit Braking System is compulsory for all cars. (with the exception of historic cars).

3.2.2. Others.

A Dual Circuit Braking System is compulsory for all cars. (with the exception of historic cars) Mechanical parking brake :- All vehicles must be fitted with a mechanical parking brake which may be applied while vehicle is travelling at any speed, and must be effective on a 1 in 6 gradient.

3.3. Suspension.

Ball joints and wheel bearings must be in good condition. Shock absorber mountings must be secure.

4. WIPERS, WASHERS, HORN & MIRROR.

4.1. Saloons.

Wipers must operate at an appropriate speed over an arc of the windscreen glass, which is sufficient to give the driver an adequate view.

4.2. Rallies.

The noise level produced by the audible warning device (horn) must be greater than or equal to 97dB during at least 3 seconds, measured 7m in front of the vehicle, activation must be available to the driver and it is strongly recommended for navigator and to be reachable by either when belted / harnessed.

4.2.3. Rear view.

This shall be provided by an inside mirror (shatterproof) commanding a rear view window with at least a 10 cm vertical opening, maintained along a width of at least 50cm. However, if the straight line connecting the upper and lower edges of the rear window opening makes an angle inferior to 20° with the horizontal, the rear view must be efficiently obtained by other means (two outside mirrors or any other system of equivalent efficiency). Furthermore, all these cars must be equipped with two outside mirrors for circuit events.

5. FUEL TANK, FUEL-OIL-WATER-LINES, ELECTRICAL CABLES.

The ageing of safety tanks entails a considerable reduction in the strength characteristics after approximately five years. No bladder shall be used more than five years after the date of manufacture, unless inspected and certified by the manufacturer for a period of up to another two years. Tank, pump and fluid lines must be properly secured, routed and hoses clipped. Clear plastic hoses must not be used for fuel. Fluid lines, including fuel, oil and brake, which are fitted externally must be protected. If fluid lines are routed through the passenger compartment, they must be metal or metal braid (except containers and fluid lines for windscreen washers and intercooler sprays) and must not be situated between the rollcage and bodyshell. Electrical cables must not be situated between the rollcage and bodyshell. Other than bulkhead connections there must not be any joins in the passenger compartment. Tank Cap must not protrude beyond bodywork; if fitted in the boot, it must have a drain-off funnel with separate drain to outside. Fuel tank breathers should be in a 'U' shape configuration or vent valve fitted.

5.1. Rally, Rallysprint, Rallycross, Autocross.

The passenger compartment must not contain any fuel, oil or hydraulic pumps or filters. (This includes power steering systems). Oil reservoirs,

if fitted in a two compartment vehicle, must be rearward of the rear axle and enclosed in their own boxed-in section.

5.2. Rally.

In a 3 compartment car the fuel tank may not be located in the centre compartment. (The rear compartment may be extended into the centre compartment provided the requirements of App2 15 are observed).

6. FIRE EXTINGUISHERS.

The containers must be securely floor mounted. In all cases their mountings must be able to withstand a deceleration of 25g. Two screw locked metal fastening straps, as per FIA standards must be fitted. If an other system is fitted, it may be retained, but the apparatus below must also be carried. It is recommended that the regulations described below are taken as a minimum and competitors are free to use additional equipment that have retaining mechanisms which comply as outlined below.

6.1 Race.

6.1.1. Single Seaters, Uno, Fiat Punto and Abarth saloon, Stryker Formula Sheane, Formula VEE and Saloon cars. Plumbed in 1.75 minimum AFFF or FIA approved equivalent including previous FIA standard, Viro3, Powder, DuPont FE-36.

6.1.2. All others (Excluding Rallies).

1 Handheld 1.75 AFFF or FIA approved equivalent including previous FIA standard, Viro3, Powder, DuPont FE-36.

6.1.3. Where a handheld extinguisher is fitted it must be accessible to the driver when normally seated.

6.1.4. Permitted extinguishants.

As per 6.2 below.

6.2. All Rallies.

6.2.1. All cars must be equipped with an extinguishing system from FIA technical list No16 (must be suitable for type of vehicle): "Extinguisher systems homologated by the FIA" (Available from www.fia.com). All extinguishers must be adequately protected and may not be fitted forward of either crew members feet (centrally mounted is allowed). Exceptions as approved by TAG are Metro 6R4 and Darrian. Any other exceptions will be notified in the MI Bulletin. In all cases their mountings must be able to withstand a deceleration of 25 g. Steel bolts and washers must be used for mounting (no aluminium rivets) All extinguishing equipment must withstand fire. Plastic pipes are prohibited and metal pipes are obligatory.

6.2.2. Both occupants must be able to trigger (and for electrically operated system, arm) the plumbed extinguisher when seated normally with their safety belts fastened and the steering

wheel in place. All systems must be armed before departure from Stage Arrival Control. Furthermore a means of triggering from the outside must be combined with the circuit breaker switch, or situated close to it. It must be marked with letter "E" in red inside a white circle of at least 10 cm diameter with a red edge.

6.2.3. The system must work in all positions.

6.2.4. Extinguisher nozzles must be suitable for the extinguishant and be installed in accordance with the manufacturers instructions. Nozzles should not point directly at the occupants.

6.3. Manual extinguishers.

6.3.1. All cars must be fitted with one or two fire extinguishers. (minimum sizes are specified below).

6.3.2. Permitted extinguishants: AFFF, FX G-TEC, Viro3, Powder, or any other extinguishant homologated by the FIA.

6.3.3. Minimum quantity of extinguishant.

AFFF: 2.4 litres
FX G-TEC: 2.0 kg
Viro3: 2.0 kg
Zero 360: 2.0 kg
Powder: 2.0kg

6.3.4. All extinguishers must be pressurized according to the contents. AFFF: FX G-TEC, Viro3: and Zero 360: in accordance with the manufacturer's instructions Powder: 8 bar minimum, 13.5 bar maximum. Furthermore, each extinguisher when filled with AFFF must be equipped with a means of checking the pressure of the contents.

6.3.5. The following information must be visible on each extinguisher: - capacity - type of extinguishant - weight or volume of the extinguishant - date the extinguisher must be checked, which must be no more than two years after either the date of filling or the date of the last check.

6.3.6. All extinguishers must be adequately protected. Their mountings must be able to withstand a deceleration of 25 g. Furthermore, only quick-release metal fastenings, with TWO metal straps, will be accepted.

6.3.7. The extinguishers must be easily accessible for the driver and the co-driver.

6.4.1. Historic cars plumbed in with one handheld complying with MSA Yearbook **R49**.

6.4.2. Rallycross and Autocross.

1 Handheld 1.75 minimum AFFF or FIA equivalent including previous FIA standard, Viro3, Powder, DuPont FE-36.

6.5. Hillclimb/Sprint.

All Cars must be equipped with either FIA Approved handheld or plumbed in system.

6.5.1. All single seaters must be equipped with FIA approved plumbed in system. The system must be armed when in the start area and during the competition.

6.5.2. Where a handheld extinguisher is fitted it must be armed when in the start area and during the competition and accessible to the driver when normally seated.

7. SAFETY BELTS / HARNESS.

For safety reasons, please note that the homologation of the following harness, whatever their validity deadlines, of this product is withdrawn with immediate effect. TAIWAN Racing Products 3" x 2" Harness D-203 T/98.

7.1. Navigation Trials.

Minimum Lap and Diagonal to comply with the Road Traffic Act. If competition type seats are used, then the belts must pass through the belt holes in the seats (to ensure proper contact with the occupants).

7.2. Racing, Rallies & Rallycross.

For National events current FIA standard including 8853/98 and 8854/98. (Belts will remain valid for 5 years after the date shown on the relevant label).

For International events FIA rules apply. (National rules apply on national section of international events).

7.3. Rallysprint, Autocross.

Current FIA standard including 8853/98 and 8854/98. No date restrictions apply.

7.4. Hillclimbs & Sprints.

Current FIA standard including 8853/98 and 8854/98. (Belts will remain valid for 5 years after the date shown on the relevant label).

For International events FIA rules apply.

In addition for Classes 1, 2, 4, 5 & 8 (as defined in the **2013** Yearbook). No date restrictions apply on national events.

7.5. Deleted.

7.6. Fixing.

Shoulder straps to be fixed within maximum 20 degrees below horizontal plane. Belts should be replaced after a heavy accident. Mounting points to the body should be very secure. Frayed, heavily soiled seat belts will not be accepted. Seat belts must be worn by competitors correctly adjusted at all times during events.

7.7. Recommendations.

It is reminded to all drivers and scrutineers that in case of a severe crash, a car's safety belts will be stretched, and therefore lose their shock absorbing capacity for further impacts. It is therefore imperative to change any harness which has undergone a high deceleration **and in such situations the scrutineers may remove the homologation label.** It is the duty of the scrutineer to refuse any harness which is in poor condition, even if the validity limit has not yet expired, and to remove the homologation label.

7.8. Seat belts cutters (2x) must be carried in all rally cars. These must be accessible to both occupants whilst seated and belted.

8. TYRES AND WHEELS.

Wheels must not extend beyond the periphery of the bodywork on Saloons all events.

8.1. Rallies.

Cars must be presented at Scrutiny only on tyres which comply with the Road Traffic Regulations.

8.1.1.

If (Racing) slick tyres are used they must comply with the following.

8.1.2. A number of equally spaced grooves to be positioned circumferentially. These will vary according to the width of the tyre, with a minimum of three grooves irrespective of tyre width.

8.1.3. The maximum spacing between grooves or between the outer groove and the tyre shoulder will not exceed 50mm and will be such that the distance between the outermost grooves is not less than three quarters of the thread width.

8.1.4. The measurement of other than straight circumferential grooves of whatever configuration, will be through the centre line of the pattern.

8.1.5. The minimum groove width will be 2mm at all times.

8.1.6. The minimum depth of groove will be 3mm on a NEW tyre, and not less than 1.6 at any other time.

8.1.7. The unauthorised grooving of slick tyres is prohibited.

8.1.8. Racing tyres are prohibited for Rallies e.g. Tyres marked with "for Racing Purposes only" or "Not for public highway use", Buffing of tyre side walls will render unfit for use. Tyres should be sound and in roadworthy condition. The wheels are free, respecting the homologated diameter (article 801.a), and the homologated width (article 801.b) for Group N which is considered as a maximum. and as per FIA Appendix J for Group A Remould Tyres which

comply with Irish Standard 412- 1988 and/or British Standard AU 144 or their European Equivalent and carry an "H" Rating or higher may be used on Tarmac Stage Rallies.

8.1.9. The inside of the tyre (the space between the rim and the internal part of the tyre) must be filled only with air (inner tube allowable, maximum wall thickness 3mm). In other words, the use of any device allowing the tyres to maintain their performance at an internal pressure equal to or less than the atmospheric pressure, is prohibited.

8.3. **Racing.** As per individual class regulations. A 1.6mm tread depth is the minimum requirement in the case of treaded tyres.

8.3.1. Rallycross.

The complete wheel (flange, rim and tyre) must always fit inside a U shaped gauge of which the extremities are:

250mm for 2 wheel drive
200mm for 4 wheel drive

The measurement to be made on the unloaded part of the tyre, but with the car in race condition and with the driver on board. The use of tyres fitted with studs, spikes or chains is not permitted.

International events.

Slick tyres are prohibited. Grooved/moulded tyres are authorised on the basis of designs homologated by the FIA. Ref. Appendix J 279.3

9. NOISE CONTROL.

Induction and exhaust noise levels must not be offensive to the public. The exhaust system must remain in place and in operation throughout the event. A silencer must be incorporated in the system. Organising clubs may exclude any car with excessive noise.

Induction Noise: For Navigation Trials an air filter must be fitted.

Exhaust Noise: Navigation/**Endurance** Events, 100 DBA maximum at 3500rpm. (2500 for Diesel engines). Rallycross and Rallysprint, 105 DBA maximum. Rallies, (National and International) All cars used in Rallies and unless the limits imposed by the local authorities are lower, the noise level on the open road must not exceed 103 dB(A) for an engine rotation speed of 3500 rpm and 2500 rpm for diesel engines. Noise testing to be carried out in accordance with current FIA approved method of testing.

10. SECURING OF EQUIPMENT INCLUDING BATTERY.

10.1, Rally, Rallycross, Autocross, Rallysprint. Battery, tools & equipment must be firmly

secured. Scrutineers strongly advise against locating the battery in the cockpit. In the event of this being necessary, the battery must be a non-spill type and placed in a leak-proof box. In this case, the protection box must include an air intake with its exit outside the cockpit (see FIA appendix J drawings 255-10 and 255- 11). If the battery situated in the cockpit is a dry battery, it must be protected electrically by a cover which covers it completely. Where a passenger is carried it may not be mounted directly forward of their feet (i.e. not behind footrest). All removable equipment (tools, spares, etc., with the exception of belt cutters) must be mounted rearward of the seats, as per FIA regulations. Exceptions as approved by TAG are Metro 6R4 and Darrian. Any other exceptions will be notified in the MI Bulletin.

10.2. Race, Hillclimb.

Any battery situated in the cockpit must be a dry battery and must be protected electrically by a cover which covers it completely.

11. THROTTLE CLOSING.

Each carburettor must have its own return spring and an additional external spring must be fitted on the throttle linkage (butterfly shaft end).

12. WARNING TRIANGLE - ALL RALLIES.

A free standing triangle must be carried in the cockpit of each competing car.

13. L.P. GAS.

This is allowed but must conform to the Motorsport Ireland requirements; (M.S.B. No. 48, May 1983). This means double straps (i.e. 4) all in leak-proof case with Inspection plate.

14. GENERAL CONDITION.

Cars to be in a clean and tidy condition. All workmanship should be of the highest standard. Load bearing structures to be generally free from rust. All material must be in place before scrutiny. Details are included in the GCRs, Articles 116 – 120 inclusive. Scrutineers will inspect cars during any event and may recommend excluding a car that is in a unsafe condition. The Scrutineers' opinion will determine what is a clean and tidy condition.

14.1. Windscreen and windows shall bear no advertising, with the exception of a maximum 10cm high strip on the upper part of the windscreen, and, provided that this does not interfere with the visibility of the driver, an 8cm high strip on the rear window.

14.2. When an on-board television camera or other recording device is fitted, written notice that the equipment is being carried must be given to the COC and the fitting and the device must be examined by a Scrutineer during pre-event scrutiny.

15. FIRE WALLS.

15.1. Bulkhead.

A **fire-resistant** bulkhead is mandatory between fuel tanks and occupants, and between engine and occupants. If the tank is under the floor, the floor is the bulkhead. Any firewall should be of a **fire-resistant material** and sealed with **fire-resistant** sealant.

15.2. Exhausts.

Exhaust Systems on Saloon and Sports Cars must exit beyond the body work.

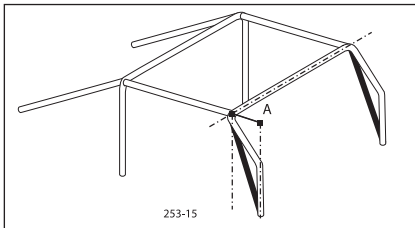
16. COMPLETE ROLL CAGE (including Intrusion Bars).

A structural framework made up of a main rollbar, a front rollbar, other optional rollbar, longitudinal braces, connections and fixation points. The cage should be constructed of cold drawn seamless carbon steel and will have minimum dimensions of 38.0mm O.D. x 2.5mm wall thickness or 40.0mm O.D. x 2mm wall thickness. All sections of the cage will have a 1/8" diameter hole drilled in them, this will be used by scrutineers to check wall thickness. All welding should be of the highest quality possible, with full penetration (preferably using gas shielded arc).

All joints must be fully welded, unless homologated otherwise. Fixation plates must be of at least 2mm thickness. The fixation of the main roll bar pillars must be done with at least 2 bolts of 8mm high tensile steel. Pillar fixation mountings must be reinforced under the body, by plates of 3mm thickness and at least 1.5 times the area of the mounting plates. The fixing of intrusion bars on both sides of the vehicle at the doors is compulsory. The tube making up this reinforcement must be welded or fixed with roll cage joint couplings to the safety roll cage and its angle with the horizontal must not exceed 15o (angled downwards towards the front of the car). No point of the bars should be higher than one third of the total height of the door measured from the base of the door. Care should be taken so the bars do not obstruct access to the front seats and do not encroach on the space provided for the driver and co-driver. Alloy cages are not permitted. Saloon Cars and Specials built or re-shelled for competition use after 1st January 1994 will require a cage as above BUT with a main hoop made of 45mm O.D. x 2.5mm wall thickness or 50mm O.D. x 2.0mm wall thickness C.D.S. steel.

Although demountable joints are permitted for the attachment of bars used in the construction of roll cages , the joint must be solid. The use of hinged joints is only allowed for door bars and removable members which do not form part of the main cage structure as per FIA Appendix J 253-2012 (Article 8 Safety Cages).

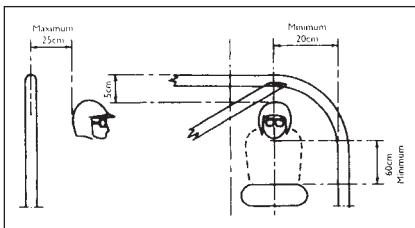
16.1. *With effect from 1 July 2011, the following cars which do not have a homologated or certified cage (or do not have the homologation papers or certificate for their cage) must have windscreen pillar reinforcement bars fitted in accordance with FIA Sporting Code Appendix J Article 253 (Ref FIA Drawing 253-15): Toyota Corolla AE86; Honda Civic Model EK; Opel/Vauxhall Corsa – B (Nova). Where a driver has concerns that he/she will be physically impeded from exiting the car in an emergency by the application of this rule, he/she may apply to Motorsport Ireland for a waiver to replace FIA Drawing 253-15 with the design shown in FIA Drawing 253-31 In all other cases the fitting of reinforcement bars to all non-homologated or uncertified cages is highly recommended.*



FIA International Sporting Code Appendix J Article 253
 8.3.2.1.4. Windscreen Pillar Reinforcement
 It must be fitted on each side of the front rollbar if dimension "A" is greater than 200 mm (drawing 253-15). It may be bent on condition that it is straight in side view and that the angle of the bend does not exceed 200. Its upper end must be less than 100 mm from the junction between the front (lateral) rollbar and the longitudinal (transversal) member (see drawing 253-52 for the measurement). Its lower end must be less than 100 mm from the (front) mounting foot of front (lateral) rollbar.

16.2. **Protective padding.**
 Where the occupants' bodies could come into contact with the safety cage, non-flammable padding must be securely fitted to the cage for protection.

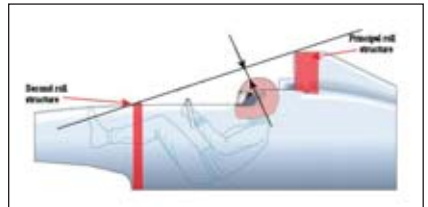
16.3. Where the occupants' crash helmets could come into contact with the safety cage, the



padding, securely fitted, must comply with FIA standard 8857-2001, type A (see technical list No. 23 on www.fia.com "Roll Cage Padding Homologated by the FIA"). A minimum distance of 15 cm between crash helmet and safety cage padding is recommended.

16.4. The upper part of the driver and co-drivers helmet must be at least 5 cm below the top of the rollbar hoop. Ref. Drawing Appendix 6.

16.5.



The gap between Drivers helmet and the line between the principal roll structure and the second roll structure as shown in the diagram above should be no less than 5cm.

17. HELMETS.

During Special Stages Rallies and all speed events competitors must wear crash helmets carrying a label issued by the relevant standards institute complying to one of the following standards:
 FIA STANDARD 8860-2004
 BS 6658-85 TYPE A /FR
 BS 6658-85 TYPE A (KART Racing Only)
 SNELL SA 2000 (USA)
 SNELL SA 2005 (USA)
 SFI 31.1A
 SFI 31.2A
 SNELL M 2000 AUTOCROSS (ONLY)
 SNELL M95 AUTOCROSS (ONLY)
 SNELL K2005 (KARTING ONLY)
 FIA CMS 2007
 FIA CMR 2007
Furthermore in open cockpit vehicles a full face helmet should be used at all times. Refer to Appendix 5 of this Yearbook for specimen labels. FIA Appendix L chapter 3 Art I (available from www.fia.com).

17.1. MI (or MSA equivalent, as long as helmet meets required MI standards listed above) Safety Helmet Approval stickers must be in position on all helmets. These tamperproof stickers will be available from selected scrutineers and will be placed on helmet as close as possible to rear of right ear position.

17.2. Painting a helmet with water base paint is permitted. Covering a helmet with material such as vinyl is not permitted.

17.3. The wearing of a head restraint approved by the FIA, e.g. HANS®, is mandatory for all drivers and co-drivers in events as follows: In all Motorsport Ireland (MI) National and International Stage Rally events (includes "00" cars).

17.4. Drivers and co-drivers of the following models of Historic Rally car are exempt from the requirement to wear a head restraint approved by the FIA, e.g. HANS®, in Stage Stage Rallies run under MI permit: Alpine A110, Alpine A310, Austin Healey Sprite, Clan Clover, Clan Crusader, Davrian, Fiat XI/9, Lancia Fulvia Zagato, Lancia Monte Carlo, Mazda RX7, MGB, MG Midget, Opel GT, Porsche 914, Triumph TR4, Triumph TR7.

18. BONNET AND BOOT FASTENINGS.

At least two additional fasteners for the front and rear bonnet and boot lid. The original fasteners having been rendered inoperative. Compulsory for Gp. A and "Modified" Rallycars. Race compulsory by removal. Strongly recommended for Gp. N Rallycars and all Saloon Racing Cars. **Bonnet must close in its original position.**

18.1. Rallies.

Bonnet and boot lid may be fibreglass / composite but must have internals / bracing similar to original bonnet / boot lid and must be fastened by way of four bonnet pins (two front and two rear), or by original type hinges and two bonnet pins **and must close in its original position.** Application: all modified rally cars.

19. WINDSCREEN/WINDOWS.

All cars must have a laminated (or FIA homologated) windscreen fitted at all times throughout the event which is clearly marked. Scrutineers will reject cars having laminated windscreens which are damaged (including delaminating) to such an extent that visibility is seriously impaired or that there is a likelihood of their breaking further during the event.

19.1. Sunstrip.

A sun strip for the windscreen is authorized, on condition that it allows the occupants to see the road signs (traffic signs, traffic lights etc). This sun strip must not cover more than one third of the depth of the visual area of the windscreen.

19.2. Anti-shatter film.

If the side windows and the glass sunroof are not made from laminated glass, the use of transparent and colourless anti-shatter films on the inside of the side windows and sunroof is mandatory. The thickness of these films must not be greater than 100 microns. but must allow the interior of the vehicle and its occupants to be visible, in normal daylight, from a distance of 5 meters. The use of silvered or tinted films can be authorised for Groups N and A ONLY (not for modified cars) in rallies only, on the side and rear

windows and on the glass sunroof, and only on the following conditions:

1. Openings in these films must allow a person outside the car to see the driver as well as the contents of the car.
2. This authorisation must be mentioned in the supplementary regulations of the event.

19.3. Polycarbonate windows.

Polycarbonate windows fitted to vehicles (not allowed in Groups A or N unless homologated) must be not be opaque but must allow the interior of the vehicle and its occupants to be visible, in normal daylight, from a distance of 5 meters.

20. HEADREST & SEATS.

20.1. Race.

As per Class regulations.

20.2. All Rallies and Rallycross.

All seats must comply with current FIA standard including 8855/99 standard (FIA technical list 12 available from www.fia.com). For safety reasons, please note that the homologation of the following competition seats, whatever their validity deadlines, is withdrawn with immediate effect:

CS.728.96 Recaro(Germany) RACER GTI
 CS.912.98 Kingdragon (Italy) Master VTR
 CS.972.99 Ektor (France) Runner 2000
 CS.980.99 Cobra (UK) Monaco
 CS.992.00 Corbeau (GB) Forza
 CS.993.00 Corbeau (GB) Pro Series
 CS.994.00 Corbeau(GB) Revolution
 CS.053.02 Corbeau (GB) Sprint
 AS.001.09 OMP Racing (Italy) HTE-One
 CS.133.05 Rossi Sports (ARG) Professional II
 CS.142.05 Rossi Sports (ARG) Professional III

As these competition seats can no longer be considered to comply with the FIA standards, their use is prohibited in all cases in which compliance with the above-mentioned standards is mandatory.

In accordance with FIA Standard 88555-1999 seats are valid for 5 years from the date (manufacture date) shown on the label. For MI National Events seats remain valid for a further 5 years after this (i.e. for MI National Events seats are valid for 10 years from the date shown on the label).

For International events FIA rules apply (National rules apply on national section of international events).

20.3. All others.

Headrest must be present for each occupant. Ref. Appendix J 253 16. Compulsory for all cars, as an integral part of the seat. No 'slip on' or adjustable varieties. High back seats are compulsory for all classes.

20.3.1. Hillclimb / Sprint.

For cars in classes 1, 2, 8 (as per 2012 Yearbook) currently homologated seats will be accepted with no date limit.

Class 4 as per HRCA & FIA Appendix K (when using currently homologated seats no date limit applies).

All other classes as 20.1 or 20.2 as applicable.

20.3.2. Rallysprint & Autocross.

Currently homologated seats will be accepted with no date limit.

20.3.3. Rally Cars.

Seats with side impact protection are highly recommended for all rally cars.

N.B. All seats must be in good condition; worn/damaged seats will not be accepted.

21. TOWING EYES.

Front and rear towing eyes which are adequate must be fitted and clearly marked and accessible to rescue services with the exception of single seaters. (Exhaust clamp not acceptable)

21.1. All Rallies.

Towing eyes must be clearly visible and painted in yellow, red or orange. (Carrying of tow rope recommended.)

22. GENERAL CIRCUIT BREAKER.

It must be operable from both inside and outside the car, clearly marked with a red spark on a white edged blue triangle. The circuit breaker must stop the engine and isolate all electrical circuits. It must operate independently to a plumbed in extinguisher system. As for the outside, the triggering system of the circuit breaker will compulsorily be situated at the lower part of the windscreen mounting for closed cars (N.S. recommended). The inside switch must be operable by all occupants when seated normally with their seat belts fastened and the steering wheel in place. This is highly recommended from 1.1.11, mandatory from 1.7.11.

23. CLOTHING AND OVERALLS.

The wearing of rain suits made from flammable material over flame retardant overalls is not permitted.

23.1. Rallies, Rallycross and Racing.

National Events: FIA homologated Flame retardant overalls, underwear, socks, boots, balaclava are compulsory (for Rallies both crew members) with gloves compulsory for drivers. Flame retardant overalls must cover the arms to the wrists, the legs to the ankles and the torso to the neck during the competitive sections of the event. Failure to wear gloves, a crash helmet, flame retardant overalls, underwear, socks, boots, balaclava and seat belts, properly fastened on the

arrival to start a stage or during a stage **may** carry the penalty of exclusion. It is mandatory that FIA approved clothing be used at all times.

23.1.1. Autocross, Hillclimb/Sprint and Rallysprint.

FIA homologated 3 layer overalls 1986 (or subsequent standard) and gloves to FIA 1986/ISO standard (or subsequent standard). All other clothing i.e. underwear (top & bottom), socks, boots & balaclava FIA 8856-2000.

For Hillclimb/Sprint; ONLY classes 1, 2, 4, 5, 8 (as defined in 2012 Yearbook) may use above. ALL other classes as per (23.1).

The overalls and gloves must be in good condition; worn/damaged items will NOT be accepted.

23.2. International Events.

FIA Approved clothing, overalls, underwear, balaclava, socks, shoes, and gloves (optional for navigator) homologated to Current FIA standard including 8856-2000 standard must be worn.

N.B. FIA Approved flame retardant underwear must be worn. The Name and Blood Group of all competitors should be embroidered (all embroidery/badges to FIA regulations.) or otherwise clearly marked on the overalls. The name and blood group on the overalls must be that of the competitor who is wearing them.

24. WINDOW WEBBING.

It is compulsory for all Saloon cars to be fitted with a protective webbing to the driver's and passengers if applicable side window opening if this window is left open during competition. This will also apply to historic cars fitted with hardtops.

25. SUNROOFS.

Glass sunroofs (if not on homologation papers) must be replaced by a panel of steel and be at least the same thickness as the remainder of the roof and be firmly secured in the closed position. Application: all groups, unless the series production fitting is maintained.

26. ADDITIONAL SAFETY INFORMATION RALLIES.

26.1. It is strongly recommended that a first aid kit is carried in both Rally car and Service vehicles.

26.2. Spare wheels, jacks, wheel braces, tools and spare parts must be securely fastened in position. Scrutineers or other officials will remove loose items from cars at stage starts.

27. LOG BOOKS.

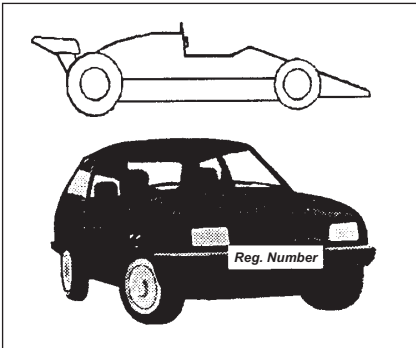
Mandatory for the following disciplines of Motor Sport.

a) Stage Rallies, Rally Prints.

- b) Historic Stage Rallies
- c) Rallycross
- d) Autocross
- e) Hillclimbs and Sprints
- f) Racing

27.1. Cars presented to scrutineers without a log book will NOT be permitted to start. Logbooks will not be issued during scrutiny. Log books must carry the current owners name and address, but if the car is hired, documentary evidence of ownership will be required.

27.2. To obtain a log book a Vehicle registration document or Tax Book (where appropriate) containing the vehicle identity number is required.



27.3. A recent colour print photograph (not polaroid) minimum 4" x 3" maximum 6" x 4" is required to be affixed to the log book. Single seater racing cars showing side elevation and saloon cars, a right hand side front three quarter view with the number plate legible (where appropriate).

27.4. Photographs will not be accepted if they are:

- Not in colour
- Polaroid
- Out of focus.

Incorrect exposure making identity difficult
 Showing wrong side of car not showing complete car
 Number plate missing (in the case of Rally cars).

27.5. Log Books must be returned to M.I. if asked for by M.I., or one of the Scrutineers listed in Art32 of Appendix 2 if any of the following alterations are required.

- Change of ownership.
- Change of address
- Change of colour (with NEW photo)
- Change of engine type or capacity

Change of Body Shell
 Vehicle Disposal or Dismantled
 Log books refer to complete cars and may not be transferred with a body shell

27.6. Cost is €40 per new/replacement Log Book, for changes of details, (as listed in Art. 27.5), €20.

27.7. Motorsport Ireland Log Books will be accepted by MSA.

27.8.1. **Appropriate** MSA Log Books accompanying vehicles held by MSA competitors will be accepted.
 27.8.2. From January 1994 Motorsport Ireland licence holders must have motorsport Ireland or FIA logbooks. Only MSA Log Books held by Motorsport Ireland competitors issued prior to January 1994 will be accepted.

27.9. To obtain a log book please contact one of the Scrutineers listed in Art 32 of Appendix 2.

27.10. **Lost Logbooks.**
 Duplicates may be issued with an entry stating duplicate and the original logbook number it is replacing.

27.11. If a vehicle does not pass scrutiny on safety grounds its log book may be retained by Motorsport Ireland, or its officials, until a satisfactory **out of competition** inspection has taken place. If a vehicle is seriously damaged as the result of an accident the log book may be requested to be returned to Motorsport Ireland, or its officials, for retention and return following an **out of competition** inspection after repairs have been carried out.

28. ELIGIBILITY.

28.1. **Homologation Papers - Group A and N Cars.**
 Homologation papers must be produced at scrutiny and available on demand throughout the event. Homologated cars must use homologated safety equipment. Note: Failure to comply will result in a car being automatically declared ineligible.

28.2. **Fuel.**
 Only fuels, in the following list, that are commercially available to be purchased by the public, may be used.

- Petrol (gasoline)
- Diesel and Bio-Diesel (Derv and Derv/bio-fuel mixtures)
- Bio-Ethanol (must conform to EN85 standard)
- Vegetable derived oils
- LPG (appropriate safety precautions to be taken)

Note: If either Bio-Ethanol or LPG are being used, the competitor must inform the organisers of this when entering the event and the vehicle must display an appropriate warning label to warn of this fact.

Particular classes or championships may limit the type or standard of fuel that can be used. This will be published in their class/championship /supplementary regulations.

No additives, which increase the octane rating of the fuel, may be used. The phrase “power boosting additive” means and has always meant any substance added to a fuel with a view to increasing engine power output. No “power boosting additives” may be used. Only air may be mixed with the fuel as an oxidant. The question of whether a given substance is or is not a component of a fuel is decided by whether such substance can or cannot be found in one of many fuels on bona-fide sale to the general public. The same applies to the proportions in which a given substance may be so found.

28.3. Comparison Rule.

Direct comparison between questionable items and production line components will be used wherever appropriate, in all classes.

28.4. Confiscation of questionable items.

The Scrutineers are empowered to confiscate any questionable component (including the logbook) they may consider necessary for further examination. Motorsport Ireland and its agents will not be held responsible for the safety of any confiscated items.

28.5. Onus for Paying Costs of Eligibility Checks.

The onus is on competitors to prove that their cars comply with the relevant regulations and the cost of any dismantling required must be borne by:

28.5.1. The competitor: when his/her car is either protested by another competitor or examined on the instructions of the organisers or Motorsport Ireland and found not to comply with the regulations.

28.5.2. The person protesting the car: if the car is found to comply with the regulations.

28.5.3. Motorsport Ireland: If Motorsport Ireland or the organisers or the Stewards of a meeting request proof of eligibility involving stripping/rebuilding, the entrant/competitor will be responsible for the full costs, whether found to be legal or not. If found to be “legal” however, the Scrutineers costs will not be added. The Clerk of the Course has the authority (Rule 163), taking all relevant factors into account to order that a deposit as determined by them to be lodged with Motorsport Ireland by the person(s) protesting. Motorsport Ireland’s function being only that of a depositor pending the outcome of the technical investigation. If on investigation the protest is found to be valid and is upheld, the deposit will be immediately returned to the person protesting.

If not upheld the deposit will not be returned until it is confirmed that the person(s) protesting has paid the expenses arising from the stripping and rebuilding and cost of the Scrutineer which may or may not exceed the deposit paid (GCR 163). N.B. Formula Vee has specified limits.

28.6 Capacity Checks.

Two camshaft cover or rocker cover and two sump (to enable sealing of the crankcase assembly) securing bolts must be cross drilled to enable wire sealing. Failure to comply will result in a car being automatically declared ineligible.

28.6.1. Where specifications apply two transmission assembly bolts to engine (to enable sealing of the flywheel assembly) and two transmission assembly bolts (to enable sealing of the transmission assembly) must be cross drilled to enable wire sealing. Failure to comply will result in a car being automatically declared ineligible.

28.7. The organising Club must provide the Scrutineers with suitable facilities to carry out post event eligibility checks.

28.8. Historic rally cars.

See Appendix 29 Articles 17 to 20.

28.9. Sealing.

28.9.1. If a vehicle or any of its components are sealed for eligibility purposes or as a result of a protest, then this fact must be entered by the sealing scrutineer in the log book relevant to the vehicle. The licensed Entrant or Driver must remain with the vehicle for the duration of the sealing process and acknowledge the examination procedure initiation by signing the issued Sealing Report. Failure to do so will result in the imposition of a fine as set out in The Schedule of Fees and Rule No.144. All subsequent vehicle eligibility examinations must be completed no later than the expiration of the forty fifth (45th) day after the sealing was carried out. Even if a vehicle, or any of its components, are sealed as a condition of competing in a championship any eligibility inspections requested must still be completed within the same time scale.

28.9.2. Failure to comply with this regulation will automatically result in the relevant vehicle being declared ineligible for the competition for which it was entered and the results for such competitions will be amended accordingly. There is no right of protest or appeal against such a declaration which will be made by the Motorsport Department. If a vehicle is so deemed to be illegal then the seals must be broken by the ‘sealing’ scrutineer or his nominee and the same vehicle/components re-sealed immediately and a further entry made in the log book.”

28.10. Forced induction intake restrictor and mountings.

These must be as per Appendix J of the current FIA Yearbook (available from www.fia.com).

Cars registered before 1st January 1996, on National events, attachment by means of two needle screws will be acceptable. The heads of both screws must be pierced so that they can be sealed. Appendix J requirements must be adhered to in all other aspects. This will be strictly enforced on all turbo charged cars.

29. SOS/OK BOARD.

An "SOS/OK" Board must be carried secured to the underside of the roof within reach of both crew members in all cars competing in rally events. This board must be made of durable material (not cardboard) 29cm x 20cm with lettering 20cm in height.

30. SPECIALS.

Autocross Specials see Appendix 87.

31. ACQUAINTANCE WITH AND SUBMISSION TO RULES.

It shall be no defense to claim no performance advantage was obtained should a vehicle be found not to comply with the regulations. (GCR 49)

31.1. All vehicles eligibility examinations must be completed no later than the expiration of the forty fifth day (45) after which the vehicle was sealed by an authorised official (the vehicle includes any individual part/s or component/s). Failure to comply with this regulation will automatically result in the relevant vehicle being declared ineligible for the competition for which it was entered and the results of such a competition will be amended accordingly. (GCR 163)

32. QUERIES.

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